



# BIHAR STATE ROAD DEVELOPMENT CORPORATION LIMITED

(A Govt. of Bihar Undertaking)

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Letter No. BSRDCL-3718/2022 - 235 (MC)

Patna, Dated : - 31/01/2022

## Addendum

Construction of 4-Lane Elevated Road as a part of JP Ganga Path (Digha to Deedarganj from Nurrudin Ghat ( Ch:16+975.79 Km) to Dharamshala Ghat (Ch:19+890 Km) (Length: 2.914 Km) with allied facilities and Widening and Strengthening of existing Road to 4-Lane from the junction of JP Ganga Path at Deedarganj(Ch:20+500Km) to ROB approach on SH-106(Old NH-30) (Length-750 Mtr.)& Development of 4-lane road from Patna Sahib Railway station (Ch. 0+000) to Patna Ghat (Ch.1+550) in replacement of existing old railway line of Patna Sahib-Patna Ghat at Patna in the State of Bihar on EPC Mode:-


Sl.No.	Clause No. / Page No.	As per RFP Document	Addendum
1	Annex I, Schedule B, Clause 5 of Vol-II Draft EPC Agreement	<p>Stretch-(B) - Flexible pavement - On existing road- minimum 50mm BC with profile correction (with BC) for camber &amp; gradient. Widening portion - the pavement composition shall be minimum 50mm BC plus 125mm DBM plus 250mm WMM plus 250mm GSB over 500mm compacted subgrade (with minimum effective CBR 10%).</p> <p>To avoid any doubt, Top of 250 mm GSB in widened portion shall match with top of existing GSB layer. The contractor for this section shall match the profile with start point at junction of JP Ganga Path end to provide smooth riding quality.</p> <p>Top 50mm BC shall be overlaid in continuity over whole existing and widened portion to act as single structural layer maintaining required camber/gradient and to provide smooth riding quality.</p>	<p>Stretch-(B) - Flexible pavement -</p> <p>(i) On existing road- Upon milling or scarifying wearing coat ; minimum 65 mm DBM with profile correction for camber &amp; gradient ; 50 mm BC shall be overlaid.</p> <p>(ii) Widening portion - The pavement composition shall be minimum 50 mm BC plus 125 mm DBM in two layers (60 mm + 65 mm) plus 250 mm WMM plus 250 mm GSB over 500 mm compacted subgrade (with minimum effective CBR 10%).</p> <p>(iii) Top 50mm BC and 65 mm DBM (2<sup>nd</sup> layer) shall be overlaid in continuity over whole existing and widened portion to act as single structural layer maintaining required camber/gradient and to provide smooth riding quality.</p> <p>Note : If any gaps remains in between 250 mm WMM and DBM 125 mm, the same shall be filled up with WMM.</p>

*[Handwritten signatures and initials]*

Sl.No.	Clause No. / Page No.	As per RFP Document	Addendum
2	Clause - 14.1, (ii) (b) Maintenance obligations of the Contractor, Vol-II Draft EPC Agreement	(b) undertaking routine maintenance including prompt repairs of potholes, cracks, joints, drains, embankments, structures, pavement markings, lighting, road signs and other traffic control devices. For the avoidance of doubt, the electricity charges for operation of electrical infrastructure installed along the project length, except the infrastructure being used by the Contractor for its own use, shall be borne by the Authority.	(b) undertaking routine maintenance including prompt repairs of potholes, cracks, joints, drains, embankments, structures, pavement markings, lighting, road signs and other traffic control devices. <ul style="list-style-type: none"> <li>The EPC contractor has to operate and maintain the electrical infrastructure installed along the project length including the street lighting and High Mast Lighting (including payment of Electricity bills) during defect Liability period0-cum-maintenance period. After completion of defect liability period-cum-maintenance period, the same shall be handed over to local body through BSRDCL.</li> <li>The Electrical charges for the infrastructure being used by the Contractor for its own use, shall also be borne by the Contractor.</li> </ul>
3	2.2.2.2 (ii) Technical Capacity, Vol-I RFP	* Similar work shall mean an Elevated Road projects/ Major Bridges/Fly Over ROB/RUB, in not less than 4-lane carriage width with at least one span of 30 meter and Experience of Pile foundation work in river for continuous stretch of 500 m minimum.	* Similar work shall mean an Elevated Road projects/ Major Bridges/Fly Over ROB/RUB, in not less than 4-lane carriage width with at least one span of 30 meter and must have the Experience of Pile foundation in ongoing or completed projects in river or sea for a minimum continuous stretch of 500 meter length in the same or other/different project.
4	Tender Drawings TCS of stretch C, PFR & Drawings, Vol-III		TCS-14 for Ch. 0+830 to 0+890 is attached as enclosure with this Addendum as Annexure-1.

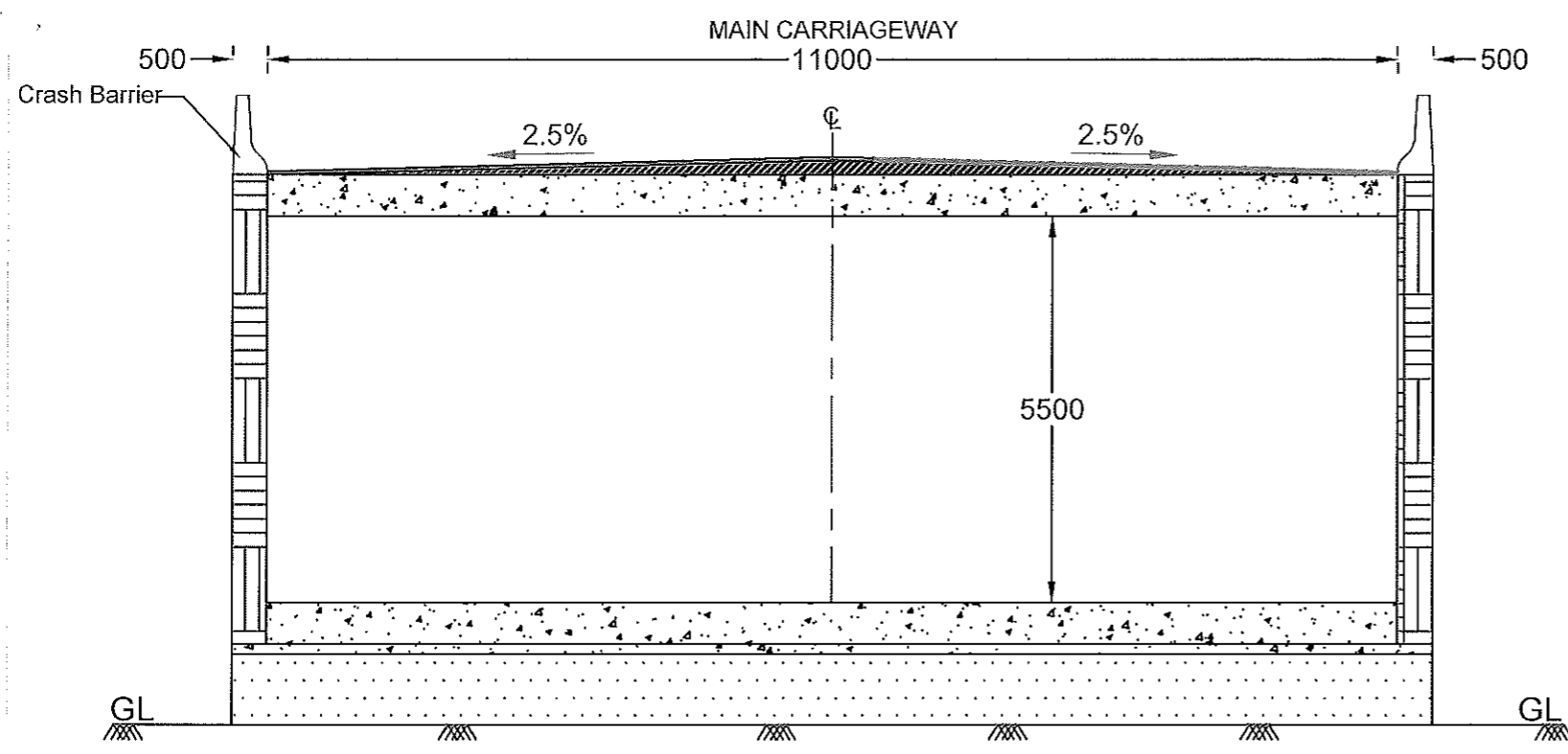
[Handwritten signatures and initials including 'R', 'SM', 'D', 'Q']

Sl.No.	Clause No. / Page No.	<p style="text-align: center;"><b>As per RFP Document</b></p> <p>(iv) In the event of withdrawal of Works under Clause 8.3 (iii) (a), the Contract Price shall be reduced by an amount equal to 90 (ninety) per cent of the value of the Works withdrawn and the Contractor shall not be entitled to any other compensation or Damages for the withdrawal of Works.</p> <p>Provided that if any Works are withdrawn after commencement of the Construction of such works, the Authority shall pay to the Contractor 110% (one hundred and ten per cent) of the fair value of the work done, as assessed by the Authority Engineer.</p> <p>The parties expressly agree that the value of the Works withdrawn shall be determined from the details available in Schedule-H. In the event that it is impossible to determine the value from Schedule-H, then the value shall be determined in accordance with the provisions of Clause 13.2 (iii).</p>	<b>Addendum</b>						
5	<p><b>Damages for delay in handing over the Site Clause - 8.3 (iv)-, Vol-II Draft EPC Agreement</b></p>		<p>(iv) In the event of withdrawal of Works under Clause 8.3 (iii) (a), the Contract Price shall be reduced by an amount equal to the percent of the value of the works withdrawn as mentioned in the Table below and the Contractor shall not be entitled to any other compensation of Damages for the withdrawal of Works.</p> <table border="1" data-bbox="454 212 845 884"> <thead> <tr> <th data-bbox="454 548 630 884">Value of the works withdrawn</th> <th data-bbox="454 212 630 548">Percentage of Value of works to be reduced from Contract</th> </tr> </thead> <tbody> <tr> <td data-bbox="630 548 702 884">Upto Rs. 100 Crore</td> <td data-bbox="630 212 702 548">90%</td> </tr> <tr> <td data-bbox="702 548 845 884">More than Rs. 100 Crore</td> <td data-bbox="702 212 845 548">Rs.90 Crore plus 95% of the amount greater than Rs. 100 Crore</td> </tr> </tbody> </table> <p>Provided that if any works are withdrawn after commencement of the Construction of such works, the Authority shall pay to the Contractor 110% (one Hundred and ten percent) of the fair value of the work done, as assessed by the Authority Engineer.</p> <p>The parties expressly agree that the value of the Works withdrawn shall be determined from the details available in Schedule-H. In the event that it is impossible to determine the value from Schedule-H, then the value shall be determined in accordance with the provisions of Clause 13.2 (iii).</p>	Value of the works withdrawn	Percentage of Value of works to be reduced from Contract	Upto Rs. 100 Crore	90%	More than Rs. 100 Crore	Rs.90 Crore plus 95% of the amount greater than Rs. 100 Crore
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More than Rs. 100 Crore	Rs.90 Crore plus 95% of the amount greater than Rs. 100 Crore								

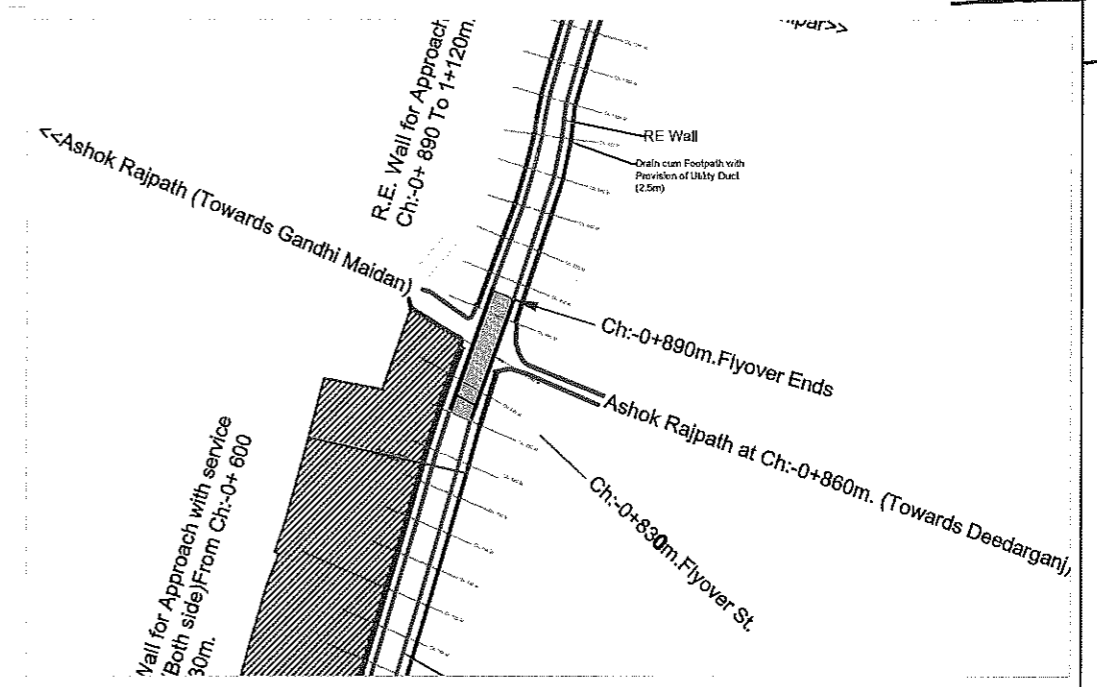
  
 (Sanjay Kumar)  
 Chief General Manager



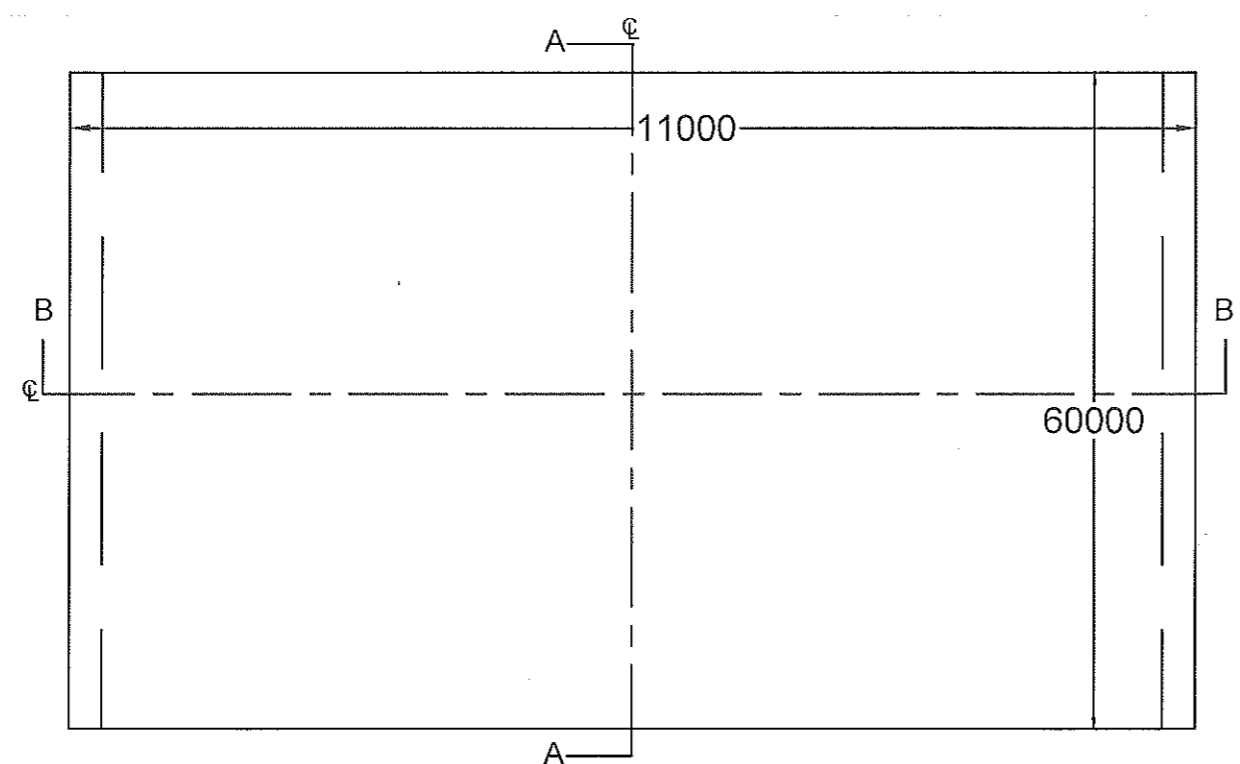




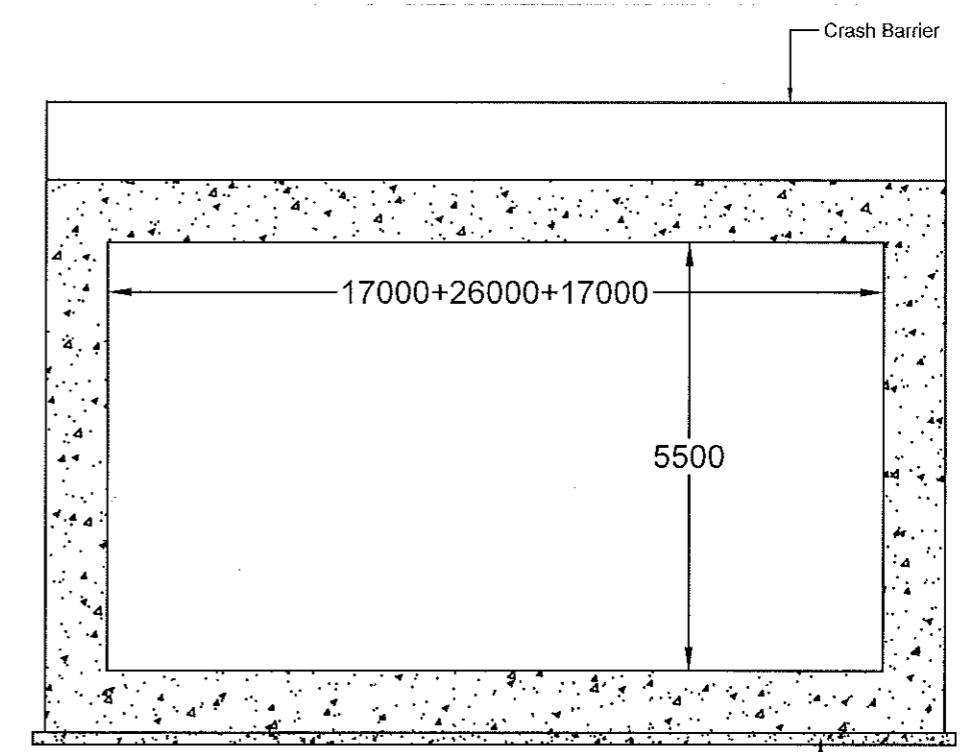
Section B-B




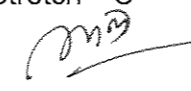
KEY PLAN



PLAN



Section A-A

 <b>PROJECT:</b>	BIHAR STATE ROAD DEVELOPMENT CORPORATION LTD. Road construction Department, Government of Bihar.	<b>TITLE</b> PROPOSED FLYOVER 0+830 to 0+890 Stretch - 'C' 	<b>AUTHORITY ENGINEER</b> AECOM - RODIC JV	TCS No. 14
	Construction of 4-Lane Elevated as a part of J.P. Ganga Path (Digha to Deeraganj from Nuruddin Ghat (Ch. 16+975.79 Km.) to Dhramshala ghat (Ch. 19+890 Km.) (length 2.914 km.) with allied facilities and Widening and Strengthening of existing Road to 4-Lane from the Junction of J.P. Ganga Path at Deearganj (Ch. 20+500 Km.) to ROB approach on SH-106 (old NH-30) (Length-750 Mtr.) & Development of 4 lane Road from Patna Sahib Railway Station (Ch.0+000) to Patna Ghat (Ch.1+550) in replacement of existing old railway line of Patna Sahib Patna Ghat at Patna in the State of Bihar on EPC mode	SCALE : NTS	DWG NO :	14
	old railway line of Patna Sahib Patna Ghat at Patna in the State of Bihar on EPC mode	DWG NO :	14	14

877

